



State of Rhode Island
Department of Administration / Division of Purchases
One Capitol Hill, Providence, Rhode Island 02908-5855
Tel: (401) 574-8100 Fax: (401) 574-8387

ADDENDUM #1

RFP #7670815

TITLE: Design Build Services for East Bay Bike Path Bridge Replacement

SUBMISSION DEADLINE: Friday April 1, 2022 at 11:00 AM

Please see the attached.

Marisa DeFarno

Marisa DeFarno
Buyer II



STATE OF RHODE ISLAND
RIDOT Addendum Notification

RFP #7670815 – DESIGN BUILD SERVICES FOR EAST BAY BIKE PATH BRIDGE REPLACEMENT

(REQUEST FOR PROPOSALS)

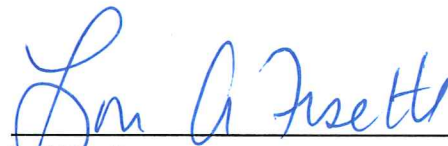
ADDENDUM #1

SUBMISSION DUE DATE: April 1, 2022 at 11:00 am

Per issuance of **ADDENDUM #1** the following revisions are noted:

1. RFP Part 1 Revised Section 8.7.1b
2. RFP Part 2 Revised Sections 3.11.4, 3.15.1, 5.3.1.2, and 7.1.3.1
3. BTC Plans Revisions to bridge typical section sheet
4. BTC Plans Addition of temporary traffic control plans
5. Mandatory Specs Revision to Liquidated Damages

APPROVED:



Lori Fisette
Acting Administrator, Project Management

1-19-22

DATE



EAST BAY BIKE PATH BARRINGTON/WARREN

BARRINGTON/WARREN, RHODE ISLAND

Bid # 7670815

BEST VALUE DESIGN-BUILD PROCUREMENT FOR
EAST BAY BIKE PATH
REQUEST FOR PROPOSALS

PART 1
INSTRUCTIONS TO PROPOSERS

ADDENDUM #1
January 18, 2022

provided material are so significant or extensive that a major revision of the Technical Proposal would be necessary with regard to this aspect of the Project.

The Technical Review Group will allocate technical points by multiplying the selected percentage rating from the scale above by the maximum number of points assigned to each of the designated subcategories listed in Section 8.6 below. Example: $P_i \times S_i = 85\% \text{ rating scale} \times \text{maximum firm experience } 6 = 5.1 \text{ Points}$.

8.6. Selection Criteria

The Technical Review Group will evaluate each Proposer's Technical Proposal and will score each Technical Proposal for all of the weighted subcategories (the "Selection Criteria") listed below:

1. Qualifications

Subcategories

- a. Firm Experience (Maximum 4 Points out of 40)
- b. Staff Qualifications (Maximum 4 Points out of 40)

2. Technical Approach

Subcategories

- a. Highway/Traffic/Staging including impacts to Vehicular, Bicycle and Pedestrian Traffic (Maximum 8 Points out of 40)
- b. Bridge, Retaining Walls, and other Structures (Maximum 4 Points out of 40)
- c. Schedule (Maximum 6 Points out of 40)
- d. Environmental Controls and Approvals (Maximum 2 Points out of 40)
- e. Overall Innovation (Maximum 6 Points out of 40)

3. Project Management

Subcategories

- a. Administration and Quality Control (Maximum 2 Points out of 40)
- b. Risk Management (Maximum 4 Points out of 40)

8.7. Determination of Overall Technical Score

The total Technical Proposal scores of each Proposer will be determined by the Technical Review Group and will then be submitted to the Department of Administration, Division of Purchases. Each set of scores for a Proposal will then be matched to the Proposer that submitted the given Proposal.

8.7.1 Minimum Selection Criteria Score for Subcategories and Minimum Overall Technical Score

- a. A proposal shall achieve Selection Criteria Scores at or above 50% of the maximum eligible score in all of the subcategories in Section 8.6 above to be considered for further evaluation; and
- b. A proposal shall achieve a minimum Overall Technical Score of 28 out of 40 in order to be considered for further evaluation.

Proposals not meeting both of the above scoring criteria will be disqualified.



EAST BAY BIKE PATH BARRINGTON/WARREN

BARRINGTON/WARREN, RHODE ISLAND

Bid # 7670815

BEST VALUE DESIGN-BUILD PROCUREMENT FOR
EAST BAY BIKE PATH
REQUEST FOR PROPOSALS

PART 2
TECHNICAL PROVISIONS

ADDENDUM #1
January 18, 2022

The need for a Transportation Management Plan (TMP) has been evaluated by the State and it has been determined that the development of a Level 3 TMP is required for this project. The D/B Entity is required to submit a TMP to the State in accordance with the FHWA guidance document titled "Developing and Implementing Transportation Management Plans for Work Zones." This document can be found at the FHWA website listed below.

http://www.ops.fhwa.dot.gov/wz/resources/publications/trans_mgmt_plans/index.htm

The D/B entity will be responsible for maintenance of traffic with regard to construction access and materials deliveries to the site, as well as maintenance of the existing pedestrian/bicycle detour that is currently in place.

Elements of the TMP shall be presented in the contract's plans (such as the Temporary Traffic Control Plan(s)) and specifications.

3.11.3. Permanent Roadside Elements

The D/B Entity shall provide pedestrian rail along the bridge approaches as recommended by AASHTO Guide for the Development of Bicycle Facilities.

New signage shall be provided as required for the proposed design in accordance with State and MUTCD standards and specifications.

3.11.4. Permanent Pavement Markings

The D/B Entity shall provide permanent pavement markings as required within the Project limits to complete the Project. The D/B Entity is required to design and install pavement markings in accordance with State and MUTCD standards and specifications.

Pedestrian crosswalk layout and details shall be constructed in accordance with the Special Provisions and according to State standards and Town of Barrington and/or Warren Standards.

3.11.5. Potential Alternatives

Alternatives may be considered and are encouraged for the design. Acceptance of these alternatives is not guaranteed. They would need to be vetted by the Proposer and submitted as an ATC as described in Part 1 of the RFP.

3.11.6. Disallowed Alternatives

ATCs that include any of the following will not be accepted by the State.

- a. Changes that will not adhere to the approved environmental documents.
- b. Changes that require additional Rights of Way, unless the D/B Entity procures said rights of way at no additional cost to the State.

3.12. Geotechnical

3.12.1. General

The D/B Entity shall conduct all work necessary to complete the geotechnical investigation and design for the project. Elements of the work shall include, but are not limited to, the following:

- a. The D/B Entity shall review the existing geotechnical information that includes the Geotechnical Data Reports (GDR, Appendix B).
- b. Additional geotechnical information in the form of historic as-built drawings and soil boring logs are included in Appendix B for reference purposes.

3.13.14. Potential Alternatives

The following alternatives may be considered for the design. These alternatives are not guaranteed to be found acceptable by the State and would need to be vetted by the D/B Entity and submitted as an ATC. This list is not all-inclusive. DB Entities are encouraged to submit other beneficial ATCs that are not listed below.

- a. Prefabricated truss
- b. Composite materials as applicable

3.13.15. Disallowed Alternatives

ATCs that include the following items will not be accepted by the State.

- a. Concrete box beams.
- b. Use of soil nail walls as a permanent retaining wall without facing.
- c. Use of soldier pile and lagging walls as a permanent retaining wall without a facing covering the soldier piles.

3.14 Architectural Elements

3.14.1. General

The general scope of the Project is based on the BTC Drawings and Special Provisions, except as modified herein. The project as a whole should be designed and constructed with cohesive treatments, including texture on retaining walls and abutments, color of paint on structural steel, etc. All architectural features must be aesthetically pleasing and be low maintenance for and be approved by RIDOT prior to construction.

3.15. Drainage Design

3.15.1. General

The D/B Entity shall inventory (and locate using GPS) the existing drainage structures within the project limits to confirm the type, size, condition, connections, inverts, etc. The BTC proposed limited changes to the pervious and impervious areas at the site. The existing drainage system shall be reviewed and modified as required to provide for the collection of stormwater. In addition, existing drainage structures and pipes may require modification and/or protection. Finish grading shall be designed to direct surface runoff away from roadway and toward structures, to the extent possible. The D/B Entity shall also be responsible for the flushing and cleaning of all pipes and drainage structures within the limits of work, and the proper disposal of all debris associated with the cleaning and flushing. Cleaning and flushing of pipes and drainage structures shall be in accordance with RIDOT Standard Specification Section 708.

The D/B Entity will be responsible for performing test pits, as needed, to meet RIDEM/CRMC permit requirements, Rhode Island Stormwater Management Design and Installation Rules (250-RICR-150-10-8), the Rhode Island Stormwater Design and Installation Standards Manual dated March 2015, and the RIDOT Linear Stormwater Manual.

Drainage design shall consider all suggestions provided in TMDL's and Stormwater Control Plans for all appropriate receiving waterbodies. The designed STUs shall be contained within the state-owned or Public right-of-way (ROW); any work outside of ROW shall be submitted as an ATC.

The D/B Entity will be responsible for treating 100% of any new/increased impervious areas which

structures as required by USCG.

5.3.1.2. Electrical Distribution

The D/B Entity will coordinate their design with both existing and proposed facilities in mind. National Grid – Electric maintains aerial electrical services within the Project area for residences and business near the Project. Throughout the life of the Project, the D/B Entity shall be responsible for maintaining power provided through these wires by protecting the wires from potential damage as needed. The D/B Entity shall perform its own investigation of the utilities to determine if the wires will be maintained/protected or relocated and shall be responsible for the design and construction associated with maintaining or relocating the wires.

The D/B Entity must notify National Grid – Electric in advance of any intended relocation of services or related electrical facilities with any work associated with the wires meeting NEC, NESC, and National Grid – Electric guidelines and practices. The existing facilities through the Project area are believed to be located as follows:

Bridge No. 083701 – There are wires parallel to the bridge along the southern/downstream side of the structure.

Bridge No. 083801 – There are wires that cross the structure. There is a pole on the north side of the west approach and a pole on the south side of the east approach.

Based on coordination with National Grid – Electric, the D/B Entity shall assume that Bridge No. 083751 and Bridge No. 083851 will each support a future 9-Way (3x3) electrical duct bank. The design of the bridges shall assume the duct bank will consist of three 6-inch conduits over six 5-inch conduits. The 6-inch and 5-inch conduit will be HW RTRC fiberglass conduit and shall be assumed to weigh 1.63 and 1.32 pounds per foot, respectively. Inside the conduits, there will be three 15 kV circuits and one 25 kV circuit. The 15 kV and 25 kV circuits will be 1000kcmil CU EPR UG cable and shall be assumed to weigh 12.6 and 16.2 pounds per foot, respectively.

5.3.1.3. Water

There are no known water facilities within the project limits.

5.3.1.4. Cable TV

There are cable television services through the Project area which may be affected by construction activities. The D/B Entity must notify the facility owner in advance of any intended relocation of services and any work associated with the facilities will meet guidelines, standards, and practices of the facility owner. The existing facilities through the Project area are believed to be located as follows:

Bridge No. 083701 – There are wires parallel to the bridge along the southern/downstream side of the structure.

Bridge No. 083801 – There are wires that cross the structure. There is a pole on the north side of the west approach and a pole on the south side of the east approach.

5.3.1.5. Telephone

There are facilities located within the Project area which may be affected by construction activities which are believed to be located as follows:

Bridge No. 083701 – There are wires parallel to the bridge along the southern/downstream

7.1.3. Technical Scheduling Requirements

All schedules shall conform to the minimum requirements, as well as those requirements outlined the D/B Entity’s Management Plan:

7.1.3.1. Standard Durations

The following standard durations shall be included in all Schedules:

Activities	Durations (calendar days)
Review of Design or Shop Drawings	30
Review of Resubmitted Design or Shop Drawing	14
RIDEM Water Quality Cert. & RIPDES Application Review	90
NBC Review	60
Category B Assent Application Review	180
Army Corps of Engineers SV or PCN Review	30
Army Corps of Engineers Individual Permit Review	120
RIPDES Remedial General Permit (RGP)	45
<u>USCG Bridge Permit</u>	<u>60</u>
DEM Waste Review – Office of Land Revitalization and Sustainable Materials Management (LRSMM)	30
Utility Design and Force Account Development	180

The following are the minimum durations to be assumed by the D/B Entity in their Proposal and Preliminary Schedules, or until Force Accounts have been received by the respective utility and may be incorporated into the Schedule. The Schedule should not represent utility relocations being performed concurrently at the same location, unless the utility companies and RIDOT accept this logic.

Minimum Utility Durations	Durations for Field Relocations (months unless otherwise noted)
NGRID – Electric	N/A
Verizon	N/A
Fiber Optic	N/A
NGRID-Gas (Tie-Ins)	N/A

STATE OF RHODE ISLAND



DEPARTMENT OF TRANSPORTATION

PLAN, PROFILE AND SECTIONS OF PROPOSED
EAST BAY BIKE PATH

REPLACEMENT OF BRIDGE NOS. 837 AND 838

EAST BAY BIKE PATH OVER BARRINGTON RIVER AND PALMER RIVER

TOWN OF BARRINGTON, TOWN OF WARREN
COUNTY OF BRISTOL

R.I. CONTRACT NO. 2022-DB-012
R.I. FAP NO. BRO-06338(002)

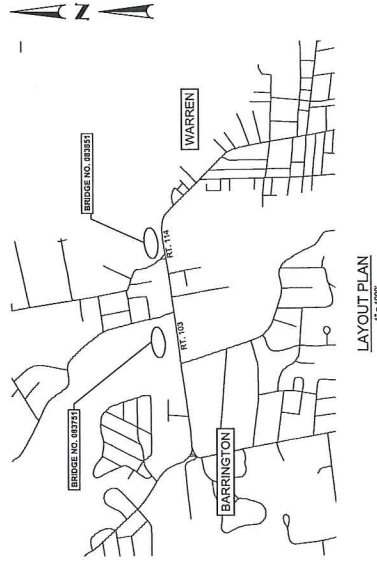
INDEX

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4	STANDARD NOTES NO. 2
5	JOB SPECIFIC NOTES AND LEGEND
6	GENERAL NOTES AND ABBREVIATIONS
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99	PIERS
100	PIERS

DESIGN NO.	PROJECT NO.	DATE	SCALE	SHEET NO.	TOTAL SHEETS
RI-2022-DB-012	BRO-06338(002)	12/2021	AS SHOWN	1	25



LOCATION MAP
1" = 500'



LAYOUT PLAN
1" = 100'

NOT FOR CONSTRUCTION

Contract Number 2022-DB-012
Number of Sheet 1
Total Sheets 25

BASE OF LEVELS
HORIZONTAL DATUM: RI STATE PLANE, NAD 83
VERTICAL DATUM: NAVD 88 * 1.30'

DECEMBER 2021

APPROVED	R.I. DEPARTMENT OF TRANSPORTATION
APPROVED	ADMINISTRATOR PROJECT MANAGEMENT
APPROVED	ADMINISTRATOR PROJECT MANAGEMENT
APPROVED	CHIEF ENGINEER OF INFRASTRUCTURE
APPROVED	CHIEF ENGINEER OF INFRASTRUCTURE
APPROVED	DIRECTOR
APPROVED	DEPARTMENT OF TRANSPORTATION
APPROVED	FEDERAL HIGHWAY ADMINISTRATION
APPROVED	DIVISION ADMINISTRATOR

R.I. STANDARD SPECIFICATIONS AND STANDARD DETAILS
SPECIFICATIONS TO COVER THIS PROJECT ARE THE R.I. STANDARD SPECIFICATIONS FOR
ROAD AND BRIDGE CONSTRUCTION, 2014 EDITION, ADOPTED MARCH 2016, AND THE STATE
STANDARD DETAILS FOR THIS PROJECT ARE R.I. STANDARD DETAILS, 1988 EDITION, WITH
ALL REVISIONS.

APPENDIX NO. 1



ARCHITECTURE
ENGINEERING
ENVIRONMENTAL
LAND SURVEYING



RHODE ISLAND
DEPARTMENT OF TRANSPORTATION

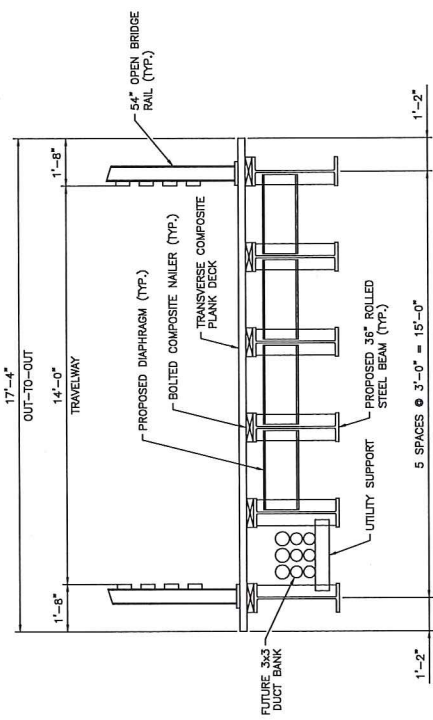
DESIGNED BY: MPE
CHECKED BY: MPE
DATE: DECEMBER 2021
SHEET 1
OF 25

NO.	DATE	BY	DESCRIPTION
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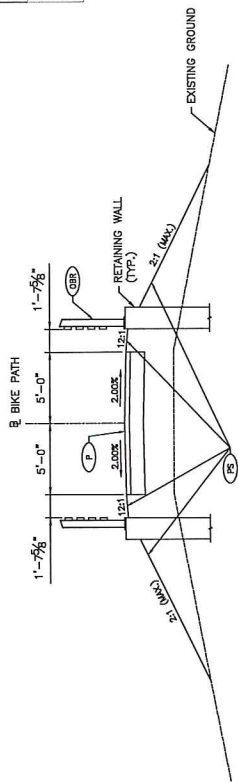
EAST BAY BIKE PATH
BRIDGE NOS. 837 & 838 REPLACEMENT
BARRINGTON/WARREN
RHODE ISLAND
COVER SHEET

REV. NO.	DATE	DESCRIPTION	BY	CHECKED	DATE
1	08/28/2021	ISSUED FOR PERMITS	RI	RI	08/28/2021

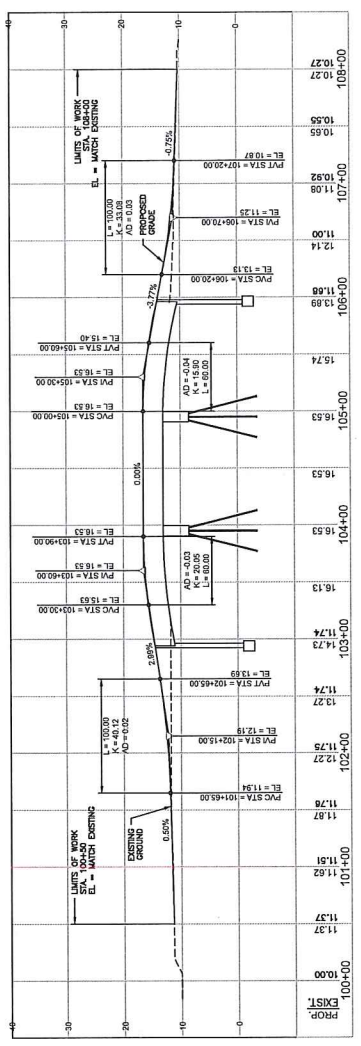
R-1



PROPOSED SECTION BRIDGE 083751
SCALE: 1/4" = 1'-0"



PROPOSED APPROACH SECTION
SCALE: 1/4" = 1'-0"



EAST BAY BIKE PATH PROFILE
HORIZ. 1" = 50'-0"
VERT. 1" = 10'-0"

- NOTES:
1. ALL STEEL SHALL BE AASHTO ASTM 270 GRADE 50 STEEL.
 2. DIMENSIONS SHOWN ARE SCHEMATIC. FINAL DIMENSIONS TO BE DETERMINED BY THE DESIGN BUILD TEAM.
 3. TIMBER, IF USED, SHALL BE PRESSURE TREATED.
 4. FUTURE 3X3 DUCT BANK SHALL CONSIST OF THREE 6" CONDUITS OVER SIX 5" CONDUITS.

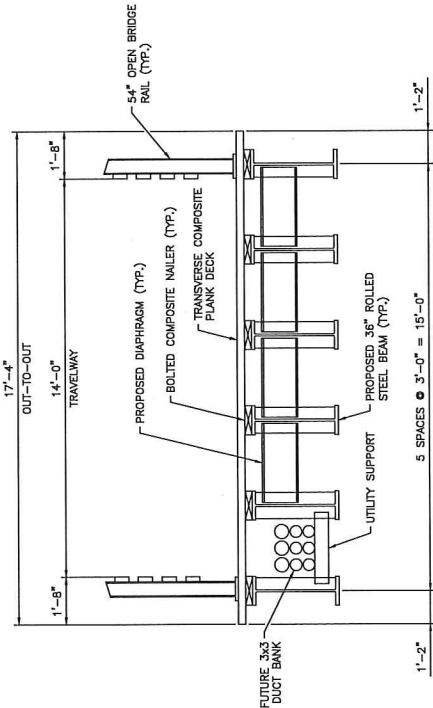
		EAST BAY BIKE PATH BRIDGE NOS. 837 & 838 REPLACEMENT BRIDGE 083751 BARRETT/OWEN/REIN	
DESIGNED BY: MFW CHECKED BY: MFW DATE: DECEMBER 2021 SHEET: 1.3 OF: 23	SCALE AS NOTED REVISIONS NO. DATE BY 1. 1/15/22 BT	TYPICAL SECTION AND PROFILE	

APPENDIX NO. 1

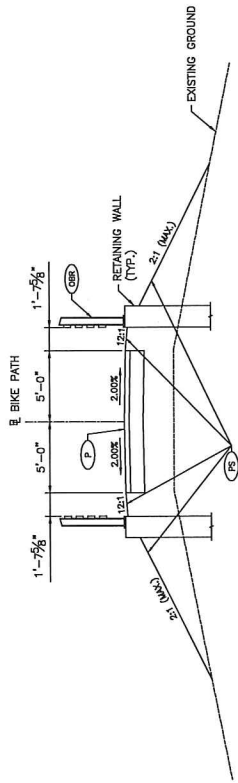
ARCHITECTURE
ENGINEERING
ENVIRONMENTAL
LAND SURVEYING
Companies

DESIGN NO.	PROJECT NO.	SCALE	DATE	BY	CHECKED BY	DATE
837-083851	837-083851	1" = 1'-0"	12/1/21	MM	MM	12/1/21

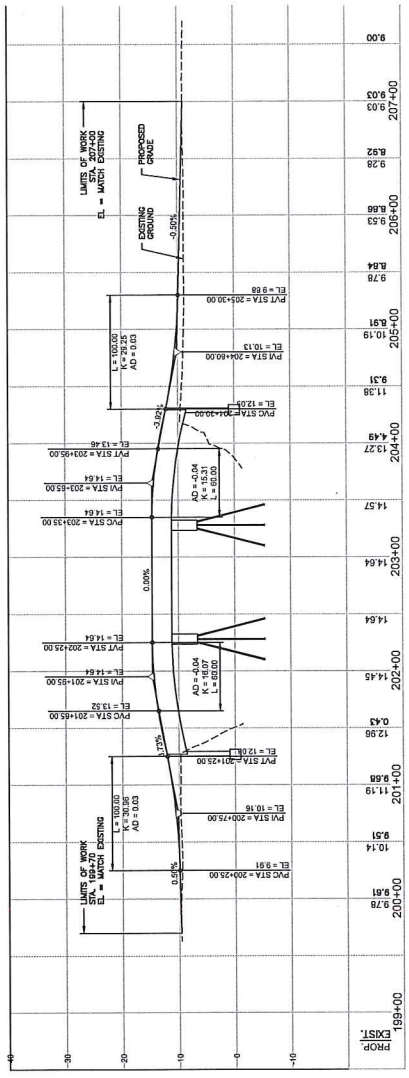
R-1



PROPOSED SECTION BRIDGE 083851
SCALE: 3/8" = 1'-0"



PROPOSED APPROACH SECTION
SCALE: 3/8" = 1'-0"



- NOTES:
1. ALL STEEL SHALL BE AASHTO ASTM 270 GRADE 50 STEEL. DIMENSIONS SHOWN ARE SCHEMATIC. FINAL DIMENSIONS TO BE DETERMINED BY THE DESIGN BUILD TEAM.
 2. TIMBER, IF USED, SHALL BE PRESSURE TREATED.
 3. FUTURE 3x3 DUCT BANK SHALL CONSIST OF THREE 6" CONDUITS OVER SIX 5" CONDUITS.

EAST BAY BIKE PATH BRIDGE NOS. 837 & 838 REPLACEMENT BARRINGTON/VANWAGEN BRIDGE 083851 RHODE ISLAND	SCALE AS NOTED	DECISION BY THE CHECKED BY: MM DATE: DECEMBER 2021 SHEET: 20 OF: 23	RHODE ISLAND DEPARTMENT OF TRANSPORTATION		ARCHITECTURE ENGINEERING ENVIRONMENTAL LAND SURVEYING BL Companies
	REVISIONS NO. DATE BY 1 12/1/21 MM	EAST BAY BIKE PATH BRIDGE 083851 REVISIONS NO. DATE BY 1 12/1/21 MM			

APPENDUM NO. 1

REV. NO.	DATE	BY	DESCRIPTION
25	05/10/02	RI	RIHQ-0308(02)
24			
23			

R-1

TRAFFIC CONTROL GENERAL NOTES

- SEE RHODE ISLAND STANDARD 281.1.1 FOR SIGN DIMENSIONS AND PLACEMENT FOR WORK ZONE TRAFFIC PILES SIGN.
- CONES SHALL BE 17 O.C. WITHIN SHOULDER AND LANE THRUERS AND TRANSITION AREA AND 20 O.C. WITHIN ALL OTHER AREAS, UNLESS OTHERWISE NOTED.
- ALL CONES SHALL CONFORM TO R.I. STANDARD 281.0. ALL DRUMS SHALL CONFORM TO R.I. STANDARD 282.0.
- 17" MINIMUM LANE WIDTHS SHALL BE MAINTAINED UNLESS OTHERWISE NOTED ON THE PLANS.
- ALL MAINTENANCE AND PROTECTION OF TRAFFIC SETUPS SHALL BE IN CONFORMANCE WITH THE 2009 EDITION OF THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES (MUTCD) AND RHODE ISLAND DEPARTMENT OF TRANSPORTATION GUIDELINES.
- ALL TRAFFIC CONTROL SETUPS SHALL BE COORDINATED WITH ADJACENT CONSTRUCTION CONTRACTORS TO DETERMINE IF SIGNS AND SETUPS SHOULD BE APPROPRIATE.
- ALL TEMPORARY CONSTRUCTION SIGNS SHALL HAVE BLACK COPY ON GRANITE BACKGROUND AND SHALL BE INSTALLED ACCORDING TO THE RHODE ISLAND STANDARDS 283.0 & 281.0.
- ALL TRAFFIC CONTROL DEVICES, UNLESS OTHERWISE NOTED, SHALL CONFORM TO HIGHWAY SIGNS, CURRENT EDITIONS. ALL SIGNS AND DRUMS SHALL BE RELECTRIFIED.
- EXISTING OR TEMPORARY SIGNS WHICH ARE IN CONFLICT WITH DAILY TRAFFIC MAINTENANCE SETUPS SHALL BE COVERED IMMEDIATELY PRIOR TO COMPLETION OF CONSTRUCTION SIGNS INSTALLED BY THE CONTRACTOR SHALL BE REMOVED OR COVERED WHEN NOT IN USE.
- WORKERS SHALL WEAR RETROREFLECTIVE PERSONAL PROTECTIVE EQUIPMENT (PPE) IN ACCORDANCE WITH THE MUTCD AND FHWA REQUIREMENTS.
- ANY CONSTRUCTION SIGNS THAT ARE PLACED BEHIND CHANNELIZING DEVICES MUST BE MOUNTED AT A HEIGHT THAT WILL MAKE THEM VISIBLE TO MOTORISTS.
- SIGNS INSTALLED ON PORTABLE STANDS REQUIRE 12 INCH MINIMUM MOUNTING HEIGHT ABOVE THE ROADWAY SURFACE TO THE BOTTOM OF THE SIGN.
- TRUCK MOUNTED ATTENUATORS (TMAs) SHALL BE USED AS GUIDES TO PREVENT TRAFFIC MANAGEMENT FOR DAILY OPERATIONS AND MAY BE MODIFIED AT THE DISCRETION OF THE ENGINEER.
- POLICE OFFICERS SHALL ASSIST WITH TRAFFIC CONTROL INCLUDING ASSISTING MOTORISTS WITH ANY OBSTRUCTED VEHICLES TO SWEET PAST THROUGH THE WORK ZONE AS NEEDED.
- TO MINIMIZE THE IMPACTS TO TRAFFIC FLOW, THE CONTRACTOR SHALL LIMIT THE WORK AREA TO THE ACTUAL LIMIT OF WORK WITHIN THE ALLOWED WORK ZONES AND SHALL MAINTAIN THE EXISTING TRAFFIC FLOW THROUGHOUT THE PROJECT.
- THE ENGINEER SHALL NOTIFY EACH USER APPROPRIATELY AS TO WORKERS IN ADVANCE OF THE START OF ANY WORK THAT WILL REQUIRE THE TEMPORARY CLOSURE OR RESTRICTION OF ACCESS.
- SAFE ACCESS AND EGRESS TO ALL DRIVEWAYS AND STREETS MUST BE MAINTAINED AT ALL TIMES UNLESS OTHERWISE APPROVED BY THE ENGINEER.
- ALL DRUMS SHALL BE APPROXIMATELY PLACED AND MOVED AS NECESSARY TO MAINTAIN TRAFFIC FLOW THROUGHOUT THE PROJECT. ALL DRUMS SHALL BE MAINTAINED THROUGHOUT THE PROJECT. ALL DRUMS AND OTHER TRAFFIC CONTROL DEVICES, GRADING AND TEMPORARY PAVEMENT PASSAGE OF PEDESTRIAN, VEHICULAR AND EMERGENCY VEHICLES THROUGHOUT THE WORK AREA, BOTH DURING AND AFTER WORK. ALL DRUMS SHALL BE MAINTAINED THROUGHOUT THE PROJECT.
- CONTRACTORS SHALL BE RESPONSIBLE FOR MAINTAINING CONSTRUCTION OPERATIONS IN ACCORDANCE WITH THE RHODE ISLAND STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION OR AS DIRECTED BY THE ENGINEER.

TAPER AND BUFFER LENGTHS

SPEED LIMIT	TAPER LENGTH (L) FEET	BUFFER SPACE FEET
25 MPH	125	135
40 MPH	320	305
55 MPH	680	485
65 MPH	780	645

MINIMUM ADVANCE WARNING SIGN SPACING

	DISTANCE BETWEEN SIGNS (FEET)		
	A	B	C
30 MPH OR LESS URBAN AREA	100	100	100
30 MPH OR GREATER URBAN AREA	350	350	350
EXPRESSWAY/FREEWAY	1,000	1,500	2,640

LEGEND

- TEMPORARY CONSTRUCTION SIGN, TYPE NOTED
- FLUORESCENT TRAFFIC CONE, R.I. STD. 281.0
- POLYETHYLENE DRUM, R.I. STD. 282.0
- FLAGGER
- WORK AREA
- TRAFFIC LANE DURING CONSTRUCTION
- NARROW CONDITION IMPACT ATTENUATOR
- PLASTIC TYPE III BARRIAGE, R.I. STD. 283.0
- POLICE OFFICER WITH CRUISER & FLASHING LIGHTS
- TRUCK MOUNTED ATTENUATOR WITH FLASHING ARROW
- PORTABLE FLASHING ARROW

ENTIRE SHEET ADDED BY ADDENDUM NO. 1

ARCHITECTURE
ENGINEERING
ENVIRONMENTAL
LAND SURVEYING

BL
Companies

RHODE ISLAND
DEPARTMENT OF TRANSPORTATION

DESIGNED BY: T.M.L.
CHECKED BY: M.F.A.
DATE: DECEMBER 2001
SHEET: 25A
OF: 25

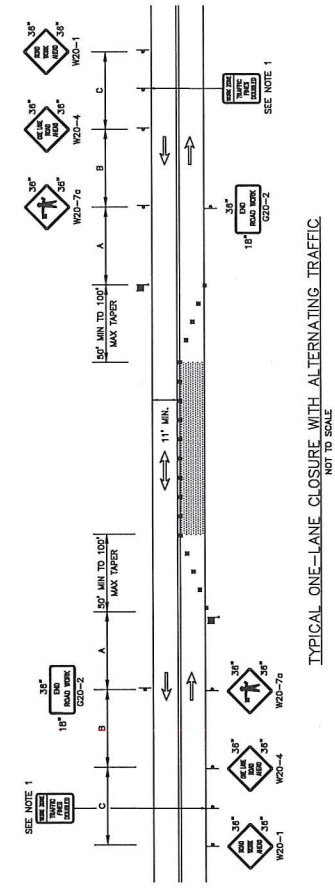
EAST BAY BIKE PATH
TRAFFIC

BARRETTOWN WARREN

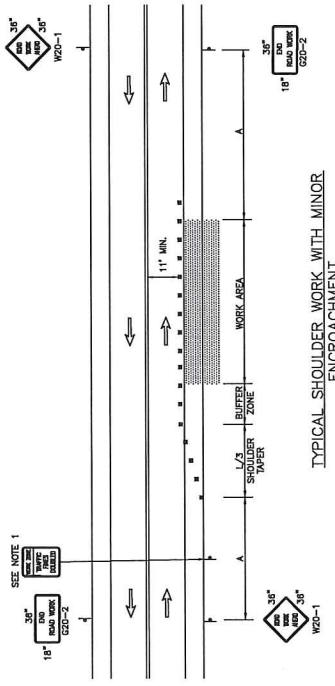
RHODE ISLAND

TTCP TYPICALS 1

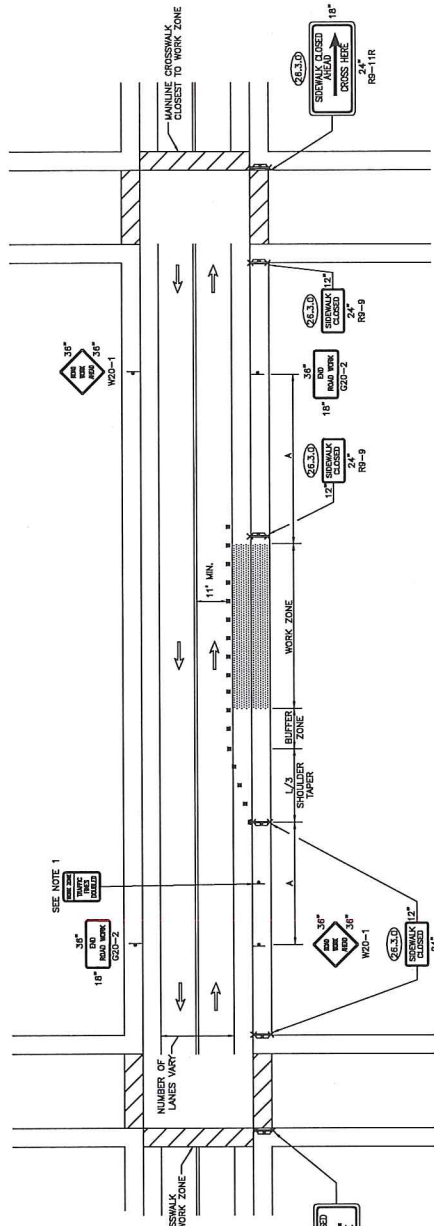
ADDENDUM NO. 1



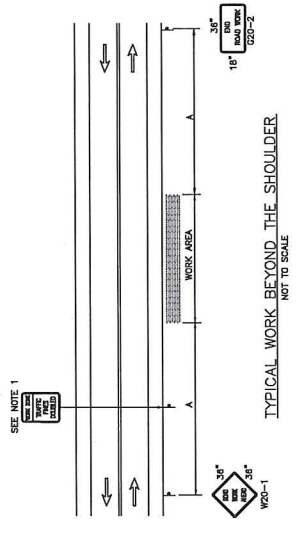
TYPICAL ONE-LANE CLOSURE WITH ALTERNATING TRAFFIC
NOT TO SCALE



TYPICAL SHOULDER WORK WITH MINOR ENCROACHMENT
NOT TO SCALE



TYPICAL SHOULDER WORK WITH MINOR ENCROACHMENT & SIDEWALK CLOSURE
NOT TO SCALE



TYPICAL WORK BEYOND THE SHOULDER
NOT TO SCALE

ENTIRE SHEET ADDED BY ADDENDUM NO. 1

	DESIGNED BY: J.A.B. CHECKED BY: M.F.A. DATE: DECEMBER 2021 SHEET: 258 OF: 25	EAST BAY BIKE PATH TRAFFIC BARRINGTON WARREN	RHODE ISLAND DEPARTMENT OF TRANSPORTATION
	ARCHITECTURE ENGINEERING ENVIRONMENTAL LAND SURVEYING BE Companies	REVISIONS NO. DATE BY DATE BY	
ADDENDUM NO. 1		TTCP TYPICALS 2	

108.1000 PROSECUTION AND PROGRESS

In accordance with **Section 108.8, Failure to Complete on Time, Para. a., Phased Completion, Interim Completion and Substantial Completion** the following defines the Interim and Substantial Completion Dates and Associated Liquidated Damages:

Substantial Completion: November 21, 2025

All Contract work shall be completed, as defined by RFP Part 3 Section 108.

Liquidated Damages: \$2,350 per Calendar Day