

RHODE ISLAND PUBLIC TRANSIT AUTHORITY

Dorrance Street Transit Center Joint Development P3

REQUEST FOR EXPRESSIONS OF INTEREST



REOI No. 22-27
May 12, 2022

ABOUT RIPTA

Created in 1964, the Rhode Island Public Transit Authority (“RIPTA”) is a body politic and corporate, vested with the power of eminent domain to acquire transit property or any interest therein. It has primary responsibility for directing statewide fixed-route bus service and ADA paratransit service operations. RIPTA is managed under the direction of a seven-member policy Board of Directors. RIPTA is committed to protecting the environment and providing safe, reliable, quality transit service that is responsive to customer needs, with particular emphasis on Rhode Island's families, children, transit dependent populations, elderly and disabled residents.

AGENCY OBJECTIVES

As the statewide public transit organization, RIPTA has a primary role to expand access and mobility opportunities for Rhode Islanders by undertaking actions and supportive strategies, directly and in collaboration with others, to provide a full range of options to the single-occupant automobile. These key mobility strategies include: (a) transit design and service strategies which help improve the livability of communities and act as a stimulus for neighborhood renewal; (b) technological advancements which increase travel options and convenience; and (c) collaborative land use strategies and economic development initiatives that foster transit ridership and pedestrian movement through transit-friendly land use and development.

MISSION STATEMENT

RIPTA’s mission is to provide safe, reliable, and cost-effective transit service with a skilled team of professionals responsive to our customers, the environment, and transit excellence.

Request for Expressions of Interest

Table of Contents

1. INTRODUCTION.....	4
1.1. Purpose of this Request for Expressions of Interest.....	4
1.2. Effect of this Request for Expressions of Interest.....	4
1.3. Eligible Entities	5
1.4. Respondent Registration	5
1.5. Submissions, Questions, and Comments.....	5
1.6. Industry Forum.....	6
1.7. Anticipated Timeline	6
1.8. No Obligation	6
2. PROJECT OVERVIEW.....	7
History and Context.	7
Project Site.	9
Potential Transaction Structure.	11
Additional Opportunity	12
Project Funding.	13
Indicative Project Concepts and Designs.	13
3. EOI CONTENT AND SUBMISSION REQUIREMENTS	14
3.1. General Submission Requirements.....	14
3.2. EOI Content.....	14
4. General Conditions	16
4.1. Changes to this REOI	16
4.2. Information Preparation Costs	16
4.3. Clarification of Responses.....	16
4.4. Disclosure of Information Contents.....	16
4.5. Ownership of Submitted Materials.....	16
4.6. Rights of RIPTA.....	17
Schedule A: Indicative Design Concept.....	18
Schedule B: Project Location.....	24
Schedule C: Socio-Demographics.....	25
Schedule D: Connections to Regional Transit	28



DORRANCE STREET TRANSIT CENTER JOINT DEVELOPMENT P3

Request for Expressions of Interest

1. INTRODUCTION

1.1. Purpose of this Request for Expressions of Interest

The Rhode Island Public Transit Authority ("RIPTA") invites interested parties to respond to this Request for Expressions of Interest ("REOI") regarding the development of the new Dorrance Street Transit Center under a joint development or public-private-partnership ("P3") arrangement.

The purpose of the REOI is to gauge initial industry interest in the development opportunity, as well as to seek industry perspective and feedback on diverse project considerations, thereby allowing for industry input on the potential joint development arrangement. Interested firms and parties are strongly encouraged to submit responses with detailed comments. The information contained in the responses to this REOI will help RIPTA steer planning and implementation efforts for this Project.

1.2. Effect of this Request for Expressions of Interest

This REOI is an inquiry only and does not constitute a formal solicitation or the initiation of a procurement process. Submissions will not be evaluated or scored. No contract or agreement will be entered into as a result of this REOI or any resulting Expression of Interest.

This REOI does not represent a commitment to issue a Request for Qualifications (RFQ) or a Request for Proposals (RFP) in the future, or a commitment that any subsequent procurement, if issued, will reflect the terms and conditions described herein.

Submission of Expressions of Interest in response to this REOI is not a prerequisite for participating in any future procurement related to the Dorrance Street Transit Center. Such participation would be subject to the criteria stipulated in subsequent solicitation documents. Participation in this REOI, or an election not to participate, will not confer on any Respondent any preference, special designation, advantage, or disadvantage whatsoever in any subsequent procurement process related to any project described herein.

Request for Expressions of Interest

1.3. Eligible Entities

RIPTA welcomes responses to this REOI from companies, corporations, consortia, or partnerships that have a potential interest in acting as lead developers, design-build contractors, operators, or equity investors for any future joint development or P3 arrangement (Respondents).

1.4. Respondent Registration

In addition to registering via Ocean State Procures™ at the State of Rhode Island Division of Purchases [www.ridop.ri.gov], individuals or firms requesting a copy of the REOI are requested to register their interest in this project by electronically submitting the following information to TransitCenterP3@ripta.com:

Company Name: _____
Contact Person: _____
Title of Contact Person: _____
Contact information: _____
Address: _____
Telephone: _____
Email: _____

Registered respondents shall be provided with updates on the Dorrance Street Transit Center in accordance with their stated contact information.

1.5. Submissions, Questions, and Comments

Respondents are asked to provide responses only to the questions indicated in Section 3 (EOI Submission Requirements), together with a 1-2 page cover letter. Respondents are NOT required to respond to all questions.

Total page count for the Response should not exceed fifteen (15) pages, utilizing 12pt Arial Narrow type font, inclusive of the cover letter. Marketing materials from Respondents are not required or encouraged as part of a written response.

Respondents should submit the requested information in electronic form (PDF format). Please enable the Commenting or User Rights Feature on the PDF documents. Adobe Professional Version 7 or above should be used for this purpose.

Request for Expressions of Interest

Respondent's submission shall be delivered by email to TransitCenterP3@ripta.com no later than 5:00 p.m. Eastern Time on **June 20, 2022**. The 'Firm Name' and 'REOI No. 22-27, Dorrance Street Transit Center' should be clearly indicated on the cover page of the EOI PDF file.

Any questions concerning this REOI should be directed by email to TransitCenterP3@ripta.com. Interpretation of this REOI or additional information will only be given by written amendment to this REOI from RIPTA. All clarifications and amendments to this REOI will be communicated via Ocean State Procures™ at the State of Rhode Island Division of Purchases website.

1.6. Industry Forum

A virtual Industry Forum is scheduled for Wednesday, June 8, 2022, at 1:00 p.m. Eastern Time. During this event, RIPTA, its advisors, and other Project sponsors will present an overview of the Project, as well as discuss opportunities and constraints around diverse delivery options. The Industry Forum is scheduled to last approximately one hour and is intended, in part, to provide an opportunity to respond to questions about this REOI and receive preliminary industry feedback prior to the EOI submission deadline. A weblink will be sent in advance of the meeting to all those registered in accordance with section 1.4 above.

1.7. Anticipated Timeline

The following timetable outlines the anticipated schedule for the REOI process. The timing and sequence of events resulting from this REOI may vary and shall ultimately be determined by RIPTA, at its sole discretion.

Anticipated Procurement Timeline	
Publish REOI	May 12, 2022
Industry Forum	June 8, 2022 at 1:00 p.m. ET
EOI Response Submission Deadline	June 20, 2022
Review of EOI / Transaction Structuring	July-August 2022
Anticipated Launch of Procurement	September 2022

1.8. No Obligation

RIPTA reserves the right to modify or terminate this REOI process at any stage if it determines, at its sole discretion, that doing so would be in its best interest. The receipt of REOI responses or other documents at any stage of either the REOI or any subsequent procurement process will in no way obligate RIPTA to proceed with a transaction or enter into any contract of any kind with any party.

2. PROJECT OVERVIEW

History and Context. The Rhode Island Public Transit Authority is Rhode Island’s statewide transit provider, serving nearly every community in the state with 53 fixed routes, on-demand zone service, and paratransit services. RIPTA provides over 16 million transit trips annually on 229 fixed-route buses. Service is centered in the Providence Metro Region, with passenger hubs located in historic city centers in Providence, Pawtucket, Warwick, and Newport.

Since 1990, Kennedy Plaza has served as the central bus depot of RIPTA’s bus network for the City of Providence. Kennedy Plaza is a public park owned by the City’s Downtown Providence Parks Conservancy (DPPC). Every year, 3 million passengers utilize Kennedy Plaza.¹ Due to transportation challenges found at Kennedy Plaza, RIPTA is proposing the development of a new intermodal facility known as the Dorrance Street Transit Center.

The Dorrance Street Transit Center is envisioned to be a mixed-use development that will include an enclosed intermodal transit center, parking deck, and RIPTA administrative offices, as well as an adjacent mixed-use transit-oriented development (TOD). In contrast to the current sprawling footprint of Kennedy Plaza, spread out in various locations across an urban park, the new Dorrance Street Transit Center will provide a single organized location.

PROJECT GOALS

- 1. Create a transit center that puts passenger needs first**
 - Make transferring easier
 - Prioritize safety and security
 - Improve passenger amenities
 - More benches
 - Restroom access
 - Expanded customer service opportunities
 - Better real-time arrival information
- 2. Improve operational efficiency**
- 3. Create a space that is manageable**
- 4. Remain centrally located**
- 5. Accommodate a growing RIPTA**
 - Planned growth per Transit Forward RI 2040
- 6. Support the State’s economy**
- 7. Create a transit experience that elevates the lives of riders**

Initially serving three million transit users annually, and considering anticipated growth,² the new state-of-the-art Dorrance Street Transit Center project is currently envisioned to entail over 27,000 square feet of new vertical construction for the intermodal transit center. The facility will host an approximately 26,860 square-foot (“sf”) indoor intermodal facility and 36,000 sf first-floor bus berth space with a parking deck potentially above the bus berth. The transit center will also feature a state-of-the-art passenger arrival and seating area with passenger amenities, RIPTA office space, and multi-modal accommodations for bicyclists and motorists. RIPTA is likewise interested in creating a public meeting space with an open-air terrace, as well as other amenities to serve the community at large. Additionally, the Project is envisioned to include a multistory, mixed-use TOD development (approximately 51,000 sf), with

¹ Based on FY19 pre-COVID ridership.

² As projected in RIPTA’s long-term transit master plan.

Request for Expressions of Interest

first-floor retail and residential space on the upper floors. The TOD development will offer improved multimodal mobility for local residents and employees, while offering economic development progress for the city. The project may be implemented on a phased basis, in which case first priority (phase 1) would be assigned to the new intermodal facility.

The Project was first conceptualized in 2014, when Rhode Island voters approved a bond referendum to improve RIPTA transit services via a new transit hub in the downtown area. Endorsed by stakeholders and gaining public support, the Dorrance Street Transit Center rose to



priority status with the adoption of RIPTA's long-range Transit Master Plan, "[Transit Forward RI 2040](#)," by the State Planning Council on December 10, 2020. Transit Forward RI 2040 outlines a detailed service expansion plan to be implemented over the next 20 years, with the relocation of the transfer hub to Dorrance Street serving as a baseline for supporting key expansion initiatives, including the following:

- Two high-capacity transit (BRT/LRT) services connecting the wider metropolitan area with Providence;
- Frequent transit service on 15 additional routes (expanded from four today);
- Offering more frequent service for longer hours on nearly all other routes;
- Extend bus service to new areas (10 new routes, extending service to 7,714 new riders);
- Adding six new rapid bus lines, serving an anticipated 5,025 new riders; and
- Fleet electrification.

Request for Expressions of Interest

Project Site.

Since 2021, RIPTA has been engaged in numerous discussions with state, local, and community leaders about the possibility of building a new, multistory, and mixed-use transit center at a site located at the intersection of Dorrance and Dyer Streets. Reception to this idea has been generally positive.

The proposed site lies between Providence's central business district and the rapidly evolving Innovation District and is preferred because of its proximity to the Downtown Transit Connector (which offers a bus every five minutes between Providence Station and Rhode Island Hospital) and because it has sufficient space to develop a facility that can both accommodate projected transit service needs and provide for mixed-used TOD value capture and commercial opportunities.

Figure 1: Project Site Location



Request for Expressions of Interest

Figure 2: Project Site Proximity to Existing Kennedy Plaza

Located in a designated **Opportunity Zone**, the Site is comprised of three contiguous parcels owned by a single entity. RIPTA has initiated efforts to secure the Site but does not yet control the Site. Pursuant to Chapter 18, General Laws of Rhode Island, RIPTA constitutes a body politic and is vested with the power of eminent domain to acquire transit property or any interest therein. Consequently, site acquisition is not viewed as a risk or impediment to advancing the Project.

Request for Expressions of Interest

Potential Transaction Structure.

As a public transportation project that will integrally relate to, and co-locate with, commercial, residential, mixed-use, and other non-transit developments, RIPTA is committed to advancing this joint development in partnership with a private developer.

Although the specifics of the transaction structure have not yet been determined, RIPTA is currently envisioning that, following a competitive procurement, a single developer will be engaged under a joint development agreement for the design, construction, financing, operation, and/or maintenance of both the intermodal transit center and the multistory, mixed-use TOD development, with RIPTA conveying the Site on the basis of a ground lease arrangement.

A phased development approach is being considered, with priority being given to the transit center. RIPTA will define output specifications and space usage requirements for the intermodal facility but seeks to leverage private sector innovation and expertise in the design and construction of the facility. RIPTA will be responsible for its own operations within the new Dorrance Street Transit Center, but the developer will be responsible for facility management and non-RIPTA operations (including commercial concessions) over the term of the agreement. Although designed, constructed, and financed by the private developer, the Dorrance Street Transit Center will be owned by RIPTA.

Compensation by RIPTA to the developer is still to be determined. As discussed below, some funding has been made available for the project, which could be used as milestone payments, offsetting a significant portion of the transit center costs. Ongoing lease or lease-like payments may also be considered, if necessary, but RIPTA anticipates that commercial revenues derived from TOD development rights and related activities will significantly (or entirely) offset the need for RIPTA payments. RIPTA welcomes feedback regarding compensation structures, as well as additional value-capture opportunities to offset its costs.

The design and operation of the multistory, mixed-use TOD development is envisioned to be a more market-based opportunity, aimed at creating a vibrant, compact, mixed-use facility adjacent to the new transit center. Although RIPTA has envisioned that the development might include first-floor retail and residential space on the upper floors, it anticipates that the private developer will design the facility based on current and projected market conditions.

RIPTA is issuing this REOI to solicit input from industry as to the optimal transaction structure. To support it in reviewing industry input and structuring a commercially viable joint development arrangement, RIPTA has recently engaged the following advisors: (i) Nixon Peabody LLP; (ii) Illuminati Infrastructure Advisors, LLC; and (iii) New Harbor Group.

Request for Expressions of Interest

Additional Opportunity.

Although not contemplated within the current scope of the Project, in order to strengthen the financial profile of the Project, there has been some consideration of expanding the scope of the Project beyond the Dorrance Street Transit Center to include additional commercial and development opportunities on three additional sites. Grant monies have been made available for the projects planned on each of these satellite sites.

- RIPTA's building in Kennedy Plaza ("the green building"):
 - The building needs to be rehabilitated and renovated;
 - The building will continue to maintain a limited transit operations presence;
 - The building has the potential to cater to retail opportunities;
 - \$1 million in grant money available for Plaza development.
- The planned transit hub on the URI-Kingston campus:
 - A building that offers interior waiting space with restrooms and passenger amenities;
 - The building is located on Plains Road in the northwest corner of campus;
 - \$5.7 million in grant money available for the planned transit hub.
- Proposed passenger building at the soon-to-be-completed Pawtucket/Central Falls Transit Center:
 - Offer an interior waiting room, with restrooms and staffed customer service space, for existing and future riders of RIPTA buses and MBTA commuter rail trains;
 - The proposed building offers retail opportunities;
 - The facility is expected to have a staffed police substation to provide site security;
 - \$5 million in grant money available for this project.

Request for Expressions of Interest

Project Funding.

Public capital funding for the Project is limited. In 2014, the voters of Rhode Island passed the \$35 million bond for transit infrastructure, of which \$21 million still remains unexpended. This funding can be leveraged toward the Project and used as milestone, lease-back, or other project-related payments. RIPTA also recently submitted to USDOT a \$29.7 million *Rebuilding American Infrastructure with Sustainability and Equity* (RAISE) grant application to help finance the construction of this facility; however, awards from the program will not be known until mid-August. RIPTA anticipates that mixed-use commercial opportunities associated with the TOD development will offset some or the rest of its Project-related costs.

It should be noted that RIPTA currently bears no credit rating, as its capital plan is financed mostly through Federal Transit Administration grants and state appropriations to leverage those federal funds. Its operating revenues are aggregated into six separate categories: (i) Federal Subsidies, (ii) State Subsidies, (iii) Other,³ (iv) Passenger Fares, (v) Third Party Fare Revenue, and (vi) Special Project Revenue.

Indicative Project Concepts and Designs.

RIPTA aims to leverage private sector design innovation for the Project. As such, limited design and engineering have been completed to date.

Although not binding on any future developer, high-level designs and renderings were produced for use in the *RAISE* grant application. Excerpts from the indicative design included in the RAISE grant application have been provided in Schedules A-D of this REOI.

RIPTA Service Planning has evaluated the potential berthing space deemed required to meet present and future transit service levels. The concept has been processed through AutoTURN (a type of vehicle path modeling software) and it has been confirmed that all turns to get into and out of the facility can be achieved.

PLEASE NOTE THAT INDICATIVE DESIGNS AND RENDERINGS INCLUDED IN SCHEDULE A OF THIS REOI ARE FOR REFERENCE ONLY AND WILL NOT BE BINDING ON ANY FUTURE DEVELOPER.

The future Developer will have maximum flexibility to design the Project in accordance with its own assessment of market conditions, as long as the public use requirements of the transit center are met.

³ Includes advertising, ID sales, rent, investments, and revenue earned from paratransit operations including for third party service requests. Other Revenue also includes miscellaneous revenue.

3. EOI CONTENT AND SUBMISSION REQUIREMENTS

3.1. General Submission Requirements

Respondents should submit an EOI in accordance with the instructions provided in this section.

To facilitate review of EOI's, Respondents are urged to be thorough but brief and, where practically possible, use tables, graphics, and diagrams as opposed to text. Respondents are requested to prepare and submit their EOI following the same structure and ordering set out in Section 3.2 (*EOI Content*) below.

Respondents are not required to provide responses to all questions, however must – at a minimum – provide the information required Section 3.2.B. (*General Information*) in order for the EOI to be considered responsive.

3.2. EOI Content

A. EOI Transmittal Letter

Please provide a cover letter, including a brief narrative, summarizing your level of interest in the Project, your potential future role in the development, and any key suggestions for making the transaction successful.

B. General Information.

- 1) Please briefly provide the following descriptive information for the Respondent:
 - (i) Name of Respondent and team members (if responding as a consortium); and
 - (ii) Describe principal business for Respondent and team members (if any).
- 2) What potential interest do you represent in relation to any eventual Joint Development or Public-Private-Partnership arrangement (e.g., lead developer, design-build contractor, equity investor, operator, or other)?
- 3) Briefly, what prior experience does the Respondent have with projects of a similar nature?

C. Transaction Structure.

- 1) In general, do you believe that the Dorrance Street Transit Center lends itself to a successful joint development public-private-partnership structure? Please explain why or why not.
 - (i) Given the stated Project objectives, what transaction structure(s) would you

Request for Expressions of Interest

suggest RIPTA consider for this project and why? Please cite at least one recent market precedent for each proposed structure (if possible).

- (ii) What do you view as some of the biggest opportunities and constraints around implementing this Project through a joint development P3 arrangement?
 - (iii) Are there any potential contractual arrangements that would discourage your potential future participation in the Project?
- 2) RIPTA is looking for opportunities to optimize the delivery of the Project using alternative finance and delivery methods, such as P3. Given your current understanding of the Project, briefly respond to the following:
- (i) Do you believe this Project, as described, is viable as a P3?
 - (ii) Do you believe the TOD development will be able to fully offset the transit facility capital and operating costs?
 - (iii) Will developers be willing to take on full TOD development risk and market risk?
 - (iv) Do you think that adding additional properties is necessary and/or beneficial to the overall financial viability of the Project?
- 3) Based on the information provided, what do you perceive as the most significant project and transaction risks?
- 4) What would be the key factors impacting your decision to pursue this Project in the future (assuming it were well structured and financially viable)?
- 5) What do you see as the advantages and disadvantages of delivering this Project under a phased approach? How might phased implementation impact the financial viability of the transaction?
- 6) How would you propose to incorporate community input into Project design?
- 7) What advantages or disadvantages would the use of a progressive P3 have for this Project?
- D. Other.**
- 1) Please provide any additional ideas or comments that may help RIPTA ensure the successful implementation of the Project.

4. General Conditions

4.1. Changes to this REOI

At any time, in its sole discretion, RIPTA may, by written amendment to this REOI, modify, amend, cancel, and/or reissue this REOI. If an amendment is issued prior to the date information is due, it will be made available to all registered Respondents in the form of an addendum.

4.2. Information Preparation Costs

RIPTA shall not be liable for any costs incurred by any Respondent in the preparation, submission, presentation, or revision of its information and response, or in any other aspect of the Respondent's pre-information submission activity. All such costs shall be borne solely by the Respondent. In no event shall RIPTA be bound by, or liable for, any obligations with respect to the Project until such time as RIPTA authorizes and executes a written agreement, and then only to the extent set forth in such agreement.

4.3. Clarification of Responses

RIPTA reserves the option, at its sole discretion, to contact a Respondent to seek clarification regarding information contained in EOI but shall have no obligation to do so. In submitting an EOI or any other form of response, a Respondent should not assume that it will be provided an opportunity to subsequently clarify or otherwise discuss any feature thereof.

4.4. Disclosure of Information Contents

Respondents submitting business or other information pursuant to this REOI should be aware that requests for RIPTA information are processed in accordance with applicable law. Respondents submitting business information pursuant to this REOI should consult Rhode Island Access to Public Records Statute, Rhode Island General Laws Title 28, Chapter 2. RIPTA may disclose submissions received in response to this REOI to advisors.

4.5. Ownership of Submitted Materials

All materials and information submitted in response to, or in connection with, this REOI shall become the property of RIPTA and will not be returned to the submitting parties. RIPTA shall

Request for Expressions of Interest

have the right to use such materials and information and ideas set forth therein without restriction or compensation to the provider.

4.6. Rights of RIPTA

RIPTA reserves to itself all rights available to it under applicable law, including but not limited to, the unqualified right, at any time and in its sole discretion, to change or modify this REOI, to reject any and all information, to seek clarification of information, to request additional information, and to undertake communications with one or more Respondents or others who, at any time subsequent to the deadline for submissions to this REOI, may express an interest in the subject matter hereof.

No Respondent, or any other person or entity, shall have any rights against RIPTA arising from the contents of this REOI, the receipt of information, or the incorporation in or rejection of information contained in any response or in any other document. RIPTA makes no representations, warranties, or guarantees that the information contained herein, or in any amendment hereto, is accurate, complete, or timely, or that such information accurately represents the conditions that would be encountered during the performance of any subsequent contract arising from a future procurement. The furnishing of such information by RIPTA shall not create, or be deemed to create, any obligation or liability upon it for any reason whatsoever. Each Respondent, by submitting its EOI, expressly agrees that it has not relied upon the foregoing information, and that it shall not hold RIPTA liable or responsible therefore in any manner whatsoever.

Schedule A: Indicative Design Concept

PLEASE NOTE THAT INDICATIVE DESIGNS AND RENDERINGS INCLUDED HEREIN ARE FOR REFERENCE ONLY AND WILL **NOT BE BINDING ON ANY FUTURE DEVELOPER.**

The new (Phase I) state-of-the-art Dorrance Street Transit Center project will entail 27,170 square feet of new vertical construction with a future (Phase II) marketable 51,000 square-foot multistory mixed-use development frontage space, both of which will be built to LEED and Energy Star standards for maximum environmental sustainability, in the hopes of a safe, new transit district and destination. The project will transfer the existing surface parking lots with dense, TOD development that will offer improved multimodal mobility for local residents and employees, while offering economic development progress for the city.

Existing Underutilized Parking Lot is the Proposed Site for the Dorrance Street Transit Center



Request for Expressions of Interest

Summary of Concept Plan for the New Dorrance Street Transit Center

Program Area	Square Feet (SF)	Description
Covered Bus Berths and vehicle circulation area	34,080	Covered, efficient, and accessible boarding and alighting areas clearly marked with the latest signage and wayfinding standards. Improved vehicle operations with opportunities for in route charging.
Passenger arrival and seating area	2,260	Fully furnished seating options with adjacent café access providing translucency from street viewing to bus arrivals and departures. Space to be secured at RIPTA's discretion. Wall along the bus location to be mostly glass, thus transparent, giving riders a visual cue when transportation has arrived. Revolving entry doors eliminate the need for vestibules as site restrictions provide a thin building ground plane footprint, with opportunities for vertical construction to contain program requirements.
Café counter	184	Food and drink dispensary offering grab-and-go food, refreshment dispersal, and premade food stuff options. Design intent to be light, airy, and open to compliment the passenger waiting area.
Ticket Booth /Information	77	Glassed-in ticket counter for manual ticket purchase of transportation. Seating and entry areas to have multiple self-service ticket kiosks for ease of passenger purchase. Real time information displays at each exterior bus berth, with interior displays to aide in passenger wayfinding. Information desk in seating area. Signage to compliment digital boards.
RIPTA Supervisor office	400	Back-of-house open office space for RIPTA supervisor, giving full viewing opportunities of bus activity within garage for improved communication and monitoring. Adjacent personal toilet rooms.
Public Toilet Rooms	463	Clean, new, durable multi-stall toilet rooms for public comfort with adjacent bottle filling station and water fountain.
Vertical circulation	492	Restricted stair and elevator locations designed to secure upper private areas from lower public circulation. Key card access for RIPTA and upper private community spaces.
Police Substation	437	Space reserved for police operations, safety monitoring, and overall security for the public. Located along the front façade with easy access to passenger seating, this space will provide full-time public safety features.
FL1 – Locker Rooms	831	RIPTA and security personnel locker rooms (male and female) with personalized lockers, mirrors, and benches for ease of use and functionality.
FL1 – Staff Break Area	1,097	Break Area to accommodate 15 occupants at a minimum (currently property allows for 20 occupants based on square footage). Amenities to include kitchenette (with refrigerator, sink microwave, casework, etc.), wall mounted TVs, exercise equipment, tables and chairs, sofas, driver information kiosk, phone charging stations with overlooks to the city through large spans of glazing.

Request for Expressions of Interest

FL1 – MEP space	1,156	Space carved out for required mechanical, electrical, and plumbing functions for the building operations.
FL1 - Vertical circulation	490	Restricted stair and elevator locations designed to secure upper private areas from lower public circulation. Key card access for RIPTA and upper private community spaces.
FL2 – Incubator Space	4,000	True business incubator startup space for the city of Providence. This space will be dedicated to the creation of new business ideas, proposals, and brick-and-mortar startups, providing wisdom and insight of past business leaders and future business mavericks. A collaborative effort to help new business minds succeed with the integration of technology, networking, information gathering and dispersal along with opportunities to gain access to business capital and seed money. A public – private partnership for the benefit of the community.
FL2 – Incubator 2	1,900	This second incubator space will be divided from the first incubator space with an operable partition to allow for both rooms to integrate into one or separate into two separate rooms with all of the functions, amenities, and technology found in the first incubator space, including adjacent private toilets. This floor is dedicated to the promotion of local business.
FL2 – Vertical Circulation	490	Restricted stair and elevator locations designed to secure upper private areas from lower public circulation. Key card access for RIPTA and upper private community spaces.
FL3 – Community Hall (enclosed)	2,924	The upper-level community hall serves as a gathering space for the public in the new RIPTA Transit center. This upper level – club like atmosphere will provide an elevated meeting space for events, corporate functions, private gatherings and other social related functions. This enclosed space will provide an operable exterior folding glass partition wall to open the room to the exterior for maximum openness.
FL3 – Community Outdoor Gathering (open air)	2,750	Adjacent to the community hall is the open-air outdoor gathering space. At the top level of the RIPTA Transit Center gatherings from the community hall can travel out to the trellised roof deck where views of the city and passersby can engage occupants. Softened with rooftop plantings, this space will be a draw for public engagement and become a destination spot for all who view and enjoy it.

Ground Floor

The ground floor of the Dorrance Street Transit Center will welcome guests from a corner entrance along Dyer Street and a sidewalk entrance on Clifford Street.

Upon entering the ground floor, riders will be greeted with an open waiting space and sleek and airy finishes, such as perforated wood ceilings, large-format tile flooring, durable and stylish solid surface materials, and video display boards relaying transportation information, news broadcasts, and associated data. Seating



Request for Expressions of Interest

will be comprised of a mixture of types, from standard airport-style ganged seating, complete with power plugs and phone charging technologies, to high-top seating for the business traveler to park and prepare for a ride. With an adjacent information desk for newcomers, the seating and waiting space will have a large span of glass for transparency and viewing opportunities into the bus berths. The revolving doors along Clifford Street and behind the transit center into the bus berths allow riders maximum egress widths with minimal occupant disturbance, eliminating the need for vestibules.

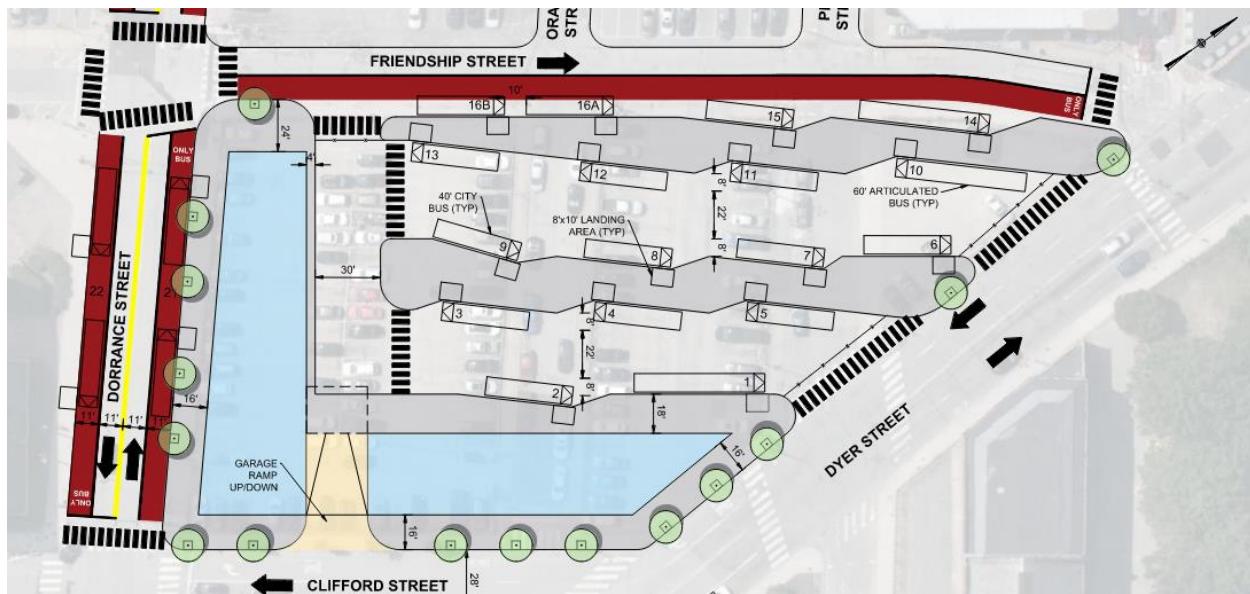
Spaces on the ground floor include a police substation, RIPTA supervisor space, ticket counter access, a café for refreshments, and necessities such as toilet rooms and water filling stations.



The bus berth area is a drastic improvement from the existing Kennedy Plaza transit area and has been designed to promote safety and an enhanced customer experience. A deck above the new berthing and alighting area provides complete protection from the elements, while creating an easy transition between the two spaces. The distance between transfers has also been greatly reduced, with berths utilizing three islands, as can be seen in the layout below. Customers will also benefit from the improvement wayfinding throughout the center, including real time bus information.

The new space has been optimized for operational efficiencies, condensed from the existing 5 acres to 0.78 acres. Contained under the overhead deck, there are 17 dedicated and fully accessible berths. The proposed sawtooth berths are oriented in both directions to provide flexibility to buses entering the site from either Friendship or Dyer Streets, and a travel lane of 22' between isles to allow vehicles to pass one another. The site has been designed for both 40' city buses and 60' articulated buses to make the turning movements into and out of the proposed center. There are also six additional berths in the immediate area along Dyer and Dorrance Streets.

Request for Expressions of Interest



First Floor

The next floor will house RIPTA's back-of-house functions. As the site dictates, a narrow footprint program will go up the building. It is envisioned that will include men's and women's locker rooms, complete with lockers, benches, and amenities needed for staff to change as needed. A staff break area will be on this floor offering amenities to allow all employees to rest, relax, and refresh. Exercise equipment, comfortable seating, such as couches and chairs, a kitchenette, and a bus driver information kiosk is also expected be included in this area.

Second Floor

The second-floor space may include incubator space for business-related operations, allowing RIPTA to partner with community business organizations for entrepreneurial benefit. It is envisioned that an operable partition will divide the floor plate into two incubator spaces, with the option to transform them into one large gathering space. Technology resources and networking opportunities will make these spaces flourish.

Request for Expressions of Interest

Third Floor

The third-floor space will be the community level. An enclosed community hall will open to an outdoor community gathering space, complete with outdoor planter and wood pergola system. An exterior operable partition can be deployed to open the enclosed space to the exterior space, allowing free flow between the indoors and outdoors. The community hall and outdoor gathering space will engage the surrounding community at the top of the Dorrance Street Transit Center, with opportunities for corporate functions, meetings, events, and other community social gatherings. This outdoor roof lounge will be a beacon to passersby, a focal point from surrounding buildings, and a visual invitation to the community to enter the building and be part of the environment.



Exterior

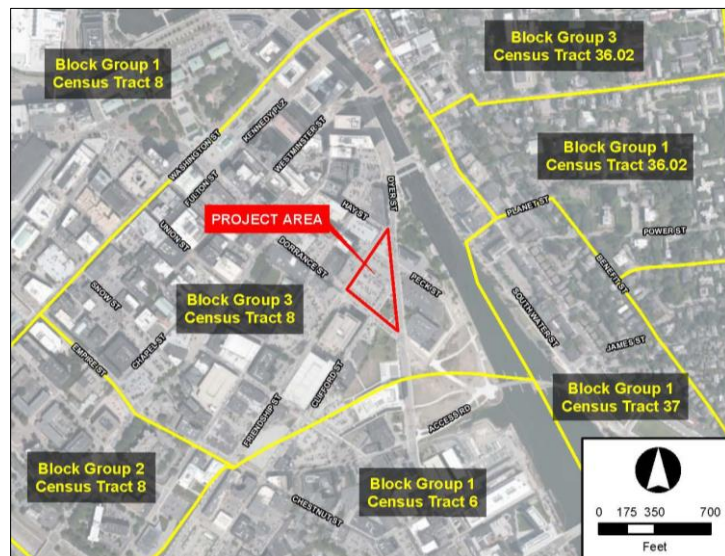
The exterior new construction will be brick veneer, fiber cement board architectural panel siding, and insulated metal panels all on metal stud wall construction. The rooftop will be a membrane roofing with rooftop pavers at the open-air outdoor gathering space, and the fenestration will consist solely of insulated aluminum curtain wall assembly with energy efficient insulated glazing (for maximum thermal efficiency). The rooftop trellis is conceptually envisioned with wood timber construction.

Schedule B: Project Location

A. Local Context

Rhode Island is a small state with a robust network of bus, commuter rail, intercity rail, and ferry services. RIPTA provides more than 16 million bus passenger trips per year, and the Rhode Island Department of Transportation (RIDOT), through a partnership with Boston's Massachusetts Bay Transportation Authority (MBTA), serves approximately 1.1 million commuter rail passenger trips a year at Providence, TF Green, and Wickford Junction Stations. Transit is the second most popular way for people to get around in Rhode Island, and it continues to be the most efficient, cost-effective, and climate-friendly way to move large numbers of people within urbanized areas. Transit is key to making Rhode Island an even more attractive place to live and work, helping the state attract businesses, connect residents with jobs, retain younger residents, and attract new talent.

RIPTA has six major transit centers: Kennedy Plaza in downtown Providence, as well as transit centers in Newport, Pawtucket, Warwick, and East Providence. Kennedy Plaza serves approximately 3 million transit users each year. As part of this project, all bus transfer activities at Kennedy Plaza will be relocated to the proposed Dorrance Street Transit Center. Dorrance Street is located in the Jewelry District, the neighborhood just south of Downtown that was historically the center of United States' jewelry manufacturing.



Project Location: 148-180 Dorrance Street

According to the U.S. Census Bureau, the project site at 148-180 Dorrance Street is located within the Providence, RI-MA, urbanized area and is **designated as an “urban” area**. According to the United States Department of Transportation, the project site is **located in an Area of Persistent Poverty and Historically Disadvantaged Community**. The project is **also located in an Opportunity Zone**. Previously, I-195 ran through the center of the Jewelry District, a barrier that stifled economic growth for decades. With the relocation of I-195 approximately 10 years ago, the Jewelry District is undergoing transformative growth and development that will add many new jobs to the city along with dense residential housing.

Schedule C: Socio-Demographics

Based on 2020 U.S. Census American Community Survey (ACS) five-year estimates, the project site is located within Census Tract 8, Block Group 3 in the City of Providence, Providence County, Rhode Island. Due to the extent of the project, Census Block Groups are best suited to report population data, as they represent a smaller geographic area.

As depicted below, the population within the project Census Block Group is 1,498.

Population Data for the Project Site, 2020

Location	Population	% of Total Population
Rhode Island	1,097,379	100%
Providence County	660,741	60%
Providence City	190,934	17%
Census Tract 8	4,646	< 1%
Block Group 3	1,498	< 1%

The 2020 ACS 5-year summary estimates from the U.S. Census Bureau were also used to determine the presence or absence of minority and/or low-income populations within the vicinity of the project site. Table 3 outlines the racial and ethnic composition surrounding the project site and for the greater region. According to the ACS estimates, approximately 51% of the population of Block Group 3 of Census Tract 8 identifies as a minority, which is 22% above the state average.

Race and Ethnicity Breakdown for the Project Site

Population	Rhode Island	Providence County	Providence City	Census Tract 8	Tract 8, Block Group 3
<i>Total Population</i>	1,097,379	660,741	190,934	4,646	1,498
Race					
<i>White</i>	79%	71%	53%	58%	64%
<i>Black/African American</i>	7%	10%	16%	13%	13%
<i>American Indian/Alaska Native</i>	0%	0%	1%	2%	1%
<i>Asian</i>	3%	4%	6%	16%	15%
<i>Pacific Islander/Native Hawaiian</i>	0%	0%	0%	0%	0%
<i>Some Other Race</i>	6%	8%	16%	5%	1%
<i>Two or more races</i>	5%	6%	8%	6%	6%
Ethnicity					
<i>Hispanic or Latino (of any race)</i>	17%	24%	44%	19%	13%
<i>Not Hispanic or Latino (of any race)</i>	83%	76%	56%	81%	87%
<i>Hispanic (White Alone)</i>	8%	11%	19%	7%	15%
Minority (Non-White and White Alone Hispanics)	29%	39%	66%	49%	51%

Source: 2016-2020 ACS 5-Year Estimates Data Profile; 2020 Decennial Census Data

Request for Expressions of Interest



Request for Expressions of Interest

The table below outlines the 2016-2020 ACS household income for the project site and the greater region. The U.S. Census Bureau states that the nationwide poverty rate for a four-person household in 2020 was \$26,200. According to the data, 27% of household incomes in Rhode Island are below the poverty level (less than \$34,999). Providence County, Providence City, Census Tract 8, and Block Group 3 of Tract 8 have 29%, 38%, 40%, and 28% of households with incomes below the poverty level, respectively, and all have median household incomes below the state averages.

Household Income for the Project Site

Household Income	Rhode Island	Providence County	Providence City	Census Tract 8	Tract 8, Block Group 3
<i>Total Households</i>	414,730	240,886	64,190	2,111	305
Median household income	\$70,305	\$62,323	\$49,065	\$45,094	-
Mean household income	\$92,427	\$81,802	\$74,501	\$95,028	-

Note: The U.S. Census Bureau does not provide household incomes for block groups.

Source: 2016-2020 ACS 5-Year Estimates Data Profile

In addition, the Environmental Protection Agency EJScreen Tool, using the 2015-2019 ACS 5-year estimates for Census Tract 8, identified that 14% of the population speaks English “*less than very well*” and that 14% of the population has less than a high school diploma.

Schedule D: Connections to Regional Transit

Providence has more RIPTA bus service than any other city or town in the state and is a hub for bus service to all major regional cities. RIPTA currently operates 229 fixed-route buses and provides special programs and lines to accommodate the mobility needs of the region:

- **Rapid Bus:** High frequency. Service every 10 minutes on weekdays. Limited stops.
 - **High Frequency:** High frequency. Every 20 minutes or less on weekdays.
 - **Local and Regional Bus:** Multiple routes operate together to offer high frequency. Every 20 minutes or less on weekdays.
 - **Express:** Long distance. High frequency. Every 30 minutes or less on weekdays.
 - **FlexZone:** Microtransit service. Board at a FlexStop without a reservation and go anywhere within the FlexZone. Call 24 hours in advance to schedule a trip from a non-FlexStop.
 - **Rural Ride:** Deviated fixed-route service. Travel between bus stops on the route without a reservation. Call 24 hours in advance for a custom pick up or drop off within 3/4 mile of a bus stop. Friday service only.
 - **Downtown Transit Connector (DTC):** Provides high-frequency transit service (every 5 minutes in each direction) between the Providence Amtrak/MBTA Station and the Hospital District in Upper South Providence.
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